



WEST LONDON LINE GROUP CALLS FOR NEW STATION

The West London Line Group is calling for **Westway Circus**, a new West London Line station, to be built underneath the roundabout at the junction of the A40(M) Westway and the A3220 (West Cross Route).

Westway Circus will be at the heart of a very high rail transport use area (Wood Lane is recorded as having the highest usage of all new stations opened in the last few years), while offering a new and extensive set of destinations not reachable from Wood Lane station.

Westway Circus users would access a wide range of destinations directly by:-

London Overground on the West London Line between Clapham Junction and Willesden Junction and on the North London Line from Willesden Junction to Stratford; and

Southern's West London Line Metro service between South Croydon and Milton Keynes

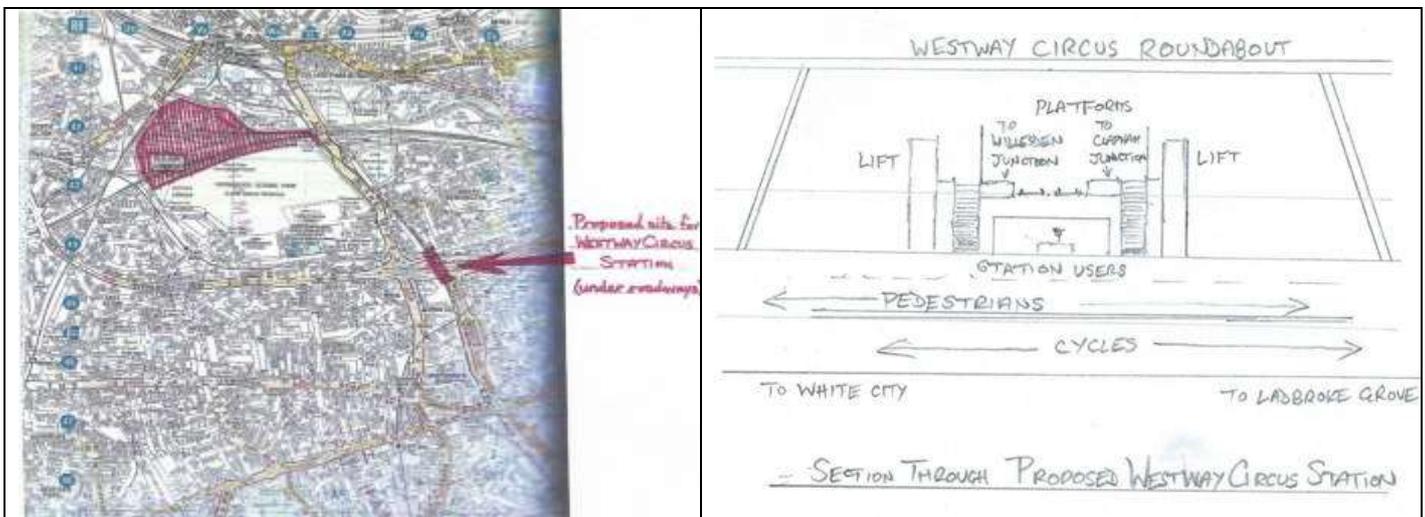
Westway Circus users would also benefit from a number of interchanges, such as Willesden Junction, Watford Junction, Milton Keynes, Clapham Junction, East Croydon and later Old Oak Common and Imperial Wharf, which between them would offer an even wider choice of destinations (see below).

Westway Circus should combine features seen elsewhere on the West London Line, viz.:-

- (i) a single ticket office below the tracks for all users – as at Imperial Wharf;
- (ii) a shared public and railway access – as at Kensington Olympia;
- (iii) eight-car platforms, with passive provision for twelve-car trains – throughout

At **Westway Circus** the public access would be used by both cycles and pedestrians, but would be overseen by railway staff in the ticket office, thus making the access more secure for non-rail users.

Westway Circus would provide the sought-after improved and secure permeability across the West London Line between the areas of the Notting Barns ward (RBKC) and that of the White City Opportunity Area known as White City East in the College Park and Old Oak ward (LBHF). With the Westway dividing both these areas into a north and south section, **Westway Circus** station will provide a new access way between – and a new public transport facility for – all four quadrants and their hinterlands, for example, the businesses under the Westway towards Ladbroke Grove.



Westway Circus - Additional Features

At **Westway Circus** there may be no need forshelters on the station platforms as these would be directly under the roundabout, although protection may be needed against air currents beneath these roadways.

Westway Circus station would be farther south than the site of the old St Quentin Park and Wormwood Scrubs station and its use would cause less annoyance to those in the rear rooms of the houses on the east and west sides respectively of Eynham and Latimer Roads than if the old site was re-used.

Westway Circus would also be of much greater use than a station on the old site, since it would serve a more extensive catchment that would include existing residences and businesses and the new White City Opportunity Area sites now under development. Moreover, with many of the latter being student accommodation, these are likely to generate proportionately higher public transport demand than similar new developments.

Westway Circus and Old Oak Common stations

The Group also seeks a new facility that would allow proper connections for WLL passengers at Old Oak Common, but this would be **in addition to the station at Westway Circus** as the two stations would be a significant distance apart and serve widely differing markets.

Interchanges (Existing)	Destinations	Interchanges (Proposed)	Destinations (Proposed) (* proposed by West London Line Group)
Shepherd's Bush	Central Line to West Ruislip, Ealing Broadway, Central and North East London	Old Oak Common	<p>Crossrail 1 to Heathrow, Maidenhead, Denham* and WCML stations to Watford Junction; also to Paddington, Central London, Canary Wharf and stations to Shenfield and Abbey Wood.</p> <p>HS2 to three Parkway stations at sites to be selected between the Chilterns and North Warwickshire*, Birmingham International, Birmingham, the North and Scotland; also to Euston for onward connections to HS1.</p> <p>GWML to Reading, the Thames Valley, the West Country and South Wales.</p> <p>Central Line to West Ruislip, Ealing Broadway, Central and North East London</p> <p>London Overground/Bakerloo Line to Euston, Harrow & Wealdstone and Watford Junction.</p> <p>London Overground to Acton Central, Gunnersbury, Kew Gardens and Richmond.</p>
Willesden Junction	London Overground/Bakerloo Line to Euston, Harrow & Wealdstone and Watford Junction London Overground to Acton Central, Gunnersbury, Kew Gardens and Richmond		
West Hampstead	Jubilee Line to Wembley Park and Stanmore, Thameslink to St Albans City, Luton and Bedford		
Gospel Oak	London Overground to Walthamstow and Barking		
Highbury & Islington	Victoria Line to Finsbury Park, Seven Sisters and Walthamstow		
Watford Junction	London Midland to St Albans Abbey Virgin Trains to the West Midlands, the North West and Scotland		
Milton Keynes	Virgin Trains to the West Midlands, the North West and Scotland		
West Brompton	District Line to Earl's Court, South Kensington, East Putney and Wimbledon		
Clapham Junction	London Overground to Peckham Rye, Surrey Quays and Canada Water Southern to South London, Gatwick, Surrey and Sussex Coast South West Trains to SW London, Surrey, Hants, Wilts, Dorset and Devon	Imperial Wharf	<p>Crossrail 2 to South West London; also Chelsea Town Hall, Sloane Square, Victoria, Central and North East London.</p> <p>London River Services to Putney, Westminster, the City and Canary Wharf.</p>
East Croydon	Tramlink to Wimbledon, New Addington, Beckenham and Elmers End		

MLB

12 December 2013



OLD OAK COMMON, HS2, CROSSRAIL, GWML, NLL AND WLL

BRIEFING NOTE – MARCH 2014

“When we build, let us think that we build for ever.” John Ruskin

The West London Line Group is very concerned that:-

- the best returns are secured from the proposed rail complex at OLD OAK COMMON to the benefit of Londoners and the users of HS2, CROSSRAIL, GWML, NLL AND WLL
- the benefits of High Speed Rail are as brought to as many UK residents as possible
- the Old Oak Common complex reduces pressure elsewhere on the road, rail and tube networks in London and across the UK.

A. Background

1. Old Oak Common is to be the nexus of the UK's two premier rail projects (Crossrail and HS2) and therefore should be as 'future-proofed' as possible. It should therefore be built to accommodate all known and forecast passenger growth to, we would suggest, the year 2080 – i.e., to avoid massive upheaval on this site in the first 50 years after the planned completion of Phase 1 of HS2.
2. Moreover, these growth figures should include significant allowance for major optimum bias as has been experienced across the whole rail network and especially with new rail developments. Relevant examples include Alloa, the South Wales Valley Lines and the latest London Overground network extensions via the East London Line to New Cross, Crystal Palace, West Croydon and now Clapham Junction. In the last instance, initial demand was three times greater than anticipated.
3. Furthermore, these instances are of just single strings of stations. At Old Oak Common there may be some additional phenomena in terms of growth patterns, given the fact that this will be a major interchange with an extraordinary range of possible journey combinations.
4. For example, in addition to the existing services on the GWML, NLL and WLL, there is the aspiration to double the Southern WLL frequency. Then there should be the new Crossrail services (we are suggesting that some Crossrail trains also use the HS2 axis to a new station at Denham Parkway to serve a potentially huge catchment across the Southern Chilterns).

5. Later will be the HS2 Phase 1 services (again potentially with our proposed three way-stations en route (catchments totaling 1.3 million) to Birmingham International (to become another of London's airports?) including an interchange with the proposed East-West Rail link).
6. Still later will be HS2 Phase 2 linking Manchester and Leeds to London (whose population will have grown by another 1.6 million to 10 million by 2030, according to Mike Brown, MD of LUL [Evening Standard 12 March 2014]).
7. Moreover, in order for the WLL and other lines to service Old Oak Common adequately, while at the same time creating additional journey combinations, we are suggesting a number of new links, including:-
 - Old Oak Common – WLL – SE London;
 - Guildford (semi-fast) – Clapham Junction – WLL – Old Oak Common – Dudding Hill Curve – St Albans – Luton (for Luton Airport);
 - Basingstoke – Staines – Feltham (for Heathrow) – Brentford – Kew Curve – Old Oak Common – NLL – Stansted Airport;
 - GWML and WCML trains via Old Oak Common and the WLL to terminate at Clapham Junction;
 - HS2 – HS1 link trains via the WLL, Clapham Junction, East Croydon, Merstham Parkway (for the M23/M25 interchange and Gatwick), Tonbridge and Ashford International
8. However, once completed, future development of this rail complex, although presently in the middle of a large open space, is likely to be severely constrained by new building above and alongside it to meet regeneration and opportunity area expectations. Therefore, this rail complex needs to be future-proofed to at least the year 2080 (and include space for a new London Overground depot that can deal with sufficient 10-car trains, if Willesden Junction cannot).
9. As a final comment in this opening section, the complex needs to work efficiently as possible throughout; therefore, it should be designed and engineered to minimise (i) rail journey times, (ii) the hassle of interchanging, and (iii) internal walking timings.
10. The desirability of even-handedness in its operation could lead to an 'Old Oak Common Tsar', to receive, review and modify designs submitted for (i) the platforms on the four axes to produce a cohesive and efficient interchange with necessary connecting lines, e.g., HS2 (to/from Denham) – Crossrail (to/from Paddington) and HS2 (to/from Denham) – WLL (to/from Westway Circus), and (ii) outside surface links to and from the (to be developed) areas surrounding the station.

B. WLLG's Concerns

In brief, our line of argument runs thus:-

11. Network Rail's Draft London & SE RUS shows an expected Capacity Utilisation of +185% on the West London Line. It is not clear as to what the scenario is/was on which this forecast was based (one- or two-way volumes; on which train formations, etc), but it seemed to exclude any impact that would arise from the RUS's expectation of interchange between the WLL, Crossrail (2018) and HS2 (2031), while putting forward a BCR of 4.2 for lengthening the WLL platforms to take 8-car trains.
12. Since then we now have virtually all of the Southern WLL trains being extended to 5- or 8-cars throughout the week as from May 2014 and the London Overground WLL trains to 5-cars by December 2015.
13. The +185% figure did not appear in the Final RUS (why not?), but the commentary still excluded any impacts of a WLL/Crossrail/HS2 interchange. (We note that at least one senior manager in Crossrail is still maintaining that Crossrail will only be building a depot and not a station at Old Oak Common.)
14. Networks north and south of the Thames are being developed to accommodate 10- or 12- car trains. So that the WLL is properly served from points south of Clapham Junction and north of Wembley/Willesden, WLL platforms should also be lengthened to accommodate 10-car (LO) and 12-car (Southern) trains. This would also include the WLL platforms at Old Oak Common.
15. Moreover, the WLLG is advocating intermediate stations on the HS2 axis between Old Oak Common and Birmingham International (Denham Parkway (Crossrail), Aylesbury/Chiltern Ridge (HS2), Claydon [for East-West Rail] (HS2) and North Warwickshire (HS2)). Denham Parkway's catchment would encompass Uxbridge, Slough, High Wycombe, Wendover, Hemel Hempstead and Watford. The local catchments of the other three stations represent a 13% increase above the combined populations of London (8 million) and Birmingham (2 million).
16. We are also advocating a new WLL station at Westway Circus both (i) to link new rail catchments (North Kensington, Ladbroke Grove, White City and the new Imperial College campuses of Imperial College West and Hammersmith Hospital) that have a very high local rail usage with a new large set of destinations, and (ii) to provide much sought-after improved pedestrian and cycle access between this proposed station's hinterlands.
17. All trains on HS2 will be stopping at Old Oak Common (11 tph in each direction). How will they be properly served – even presuming a Crossrail interchange will be provided – and ease the pressures on the five Underground lines at Euston/Euston Square, unless they can be accessed from rail and other modes in the area? These other accesses, plus the new regeneration area that will be integral to the rail complex, will generate considerable demand for all these rail services, including the WLL.
18. However, no station exists at Old Oak Common yet. Therefore, there should be no justification in attempting to exercise 'Grandfather' or 'Higher Prestige Route' rights. The complex should be designed to allow all users to benefit equally from the complex's interchange possibilities. This means that every effort should be made by all concerned (including relaxing the 'Red Lines' put forward by the DfT and/or local authorities) to ease access to and within the rail complex for the overall benefit of all passengers at Old Oak Common.

C. WLLG's Suggested Solutions

19. We see significant advantages in an independent 'Old Oak Common Tsar' being appointed to receive, review and modify designs submitted for (i) the platforms on the four axes to produce a cohesive and efficient interchange with necessary connecting lines, e.g., HS2 (to/from Denham) – Crossrail (to/from Paddington) and HS2 (to/from Denham) – WLL (to/from Westway Circus and (ii) accesses to local pedestrian, cycle, bus and other modes for the regeneration areas surrounding the rail complex.
20. Each rail axis should access each other with as little distance between them as possible to minimise the extended WLL and NLL journey times due to the sharp curves presently proposed and the walking times from their platforms presently proposed to be some considerable distance from the other axes. Otherwise, this will build in, even before the rail complex is open and, to the detriment of both the London and the UK economies many 'person-years' of non-productivity – permanently.
21. Given the relatively very low speeds of all GWML, Crossrail and HS2 trains immediately east of Old Oak Common, it should be possible to slew existing or lay new tracks to accommodate the requisite stanchions for the higher level WLL and NLL, thus reducing NLL/WLL through and interchange timings without any time penalties on the other axes. WLL timings will also be extended by the serving of Westway Circus, but this is not a case for its non-provision, given the multiplicity of rail and non-rail transportation, public safety and amenity advantages of its opening.
22. Plans for the interchange should remain largely as at present for the GWML/Crossrail and HS2 platforms, save that, to reduce the vertical distance between the latter and the WLL/NLL, the HS2 platforms should be elevated, as on the central section of the Piccadilly Line, using the resultant gradients to aid braking and acceleration. Even now, that between City Thameslink and Blackfriars (said to be 1 in 29) is efficiently negotiated by trains on the Thameslink network.
23. The WLL and NLL should invert their southern approaches to Willesden Junction, both crossing above the GWML/Crossrail/HS2 complex towards its eastern end, with new links between the western end of the WLL (12-car) platforms and (i) the Dudding Hill Curve (if necessary) and West Coast Main Line (for the WLL Southern services), and (ii) the present axis of the NLL to Willesden Junction (for the WLL LO services).
24. Similarly, the NLL should be diverted from close to Acton Wells Junction to run between the axes of GWML/Crossrail and HS2, with a two or four 10-car platform layout above the GWML/Crossrail/HS2 complex and curving back to join the present route of the WLL into Willesden Junction.
25. There should also be a link between the HS2 tracks (to/from Denham) and the WLL (to/from Westway Circus) to provide an additional link with HS1 via Clapham Junction, East Croydon, Merstham (for Gatwick and the M23/M25 junction), Tonbridge and Ashford International.
26. This would (i) allow for market growth, (ii) bring both HS networks closer to the rail and road links to south-east, southern and south-west England, and (iii) provide an alternative, diversionary HS route during times of disruption via Euston and Stratford.
27. Taking these proposed new axes for the WLL and NLL and careful design of the links in the paragraph above should mean that there would be no encroachment on areas such as Wormwood Scrubs.
28. HS2, GWML, WLL should all accommodate double-deck trains and HS2 should be wide enough to allow single-line working when needed – instead of 'bustitution' on what will be the UK's premier rail network.
29. Space should also be included at Old Oak Common for a new London Overground depot that can deal with sufficient 10-car trains, if Willesden Junction cannot.

MLB

13 March 2014



20 March 2014

SUGGESTED LAYOUT and SERVICES FOR OLD OAK COMMON FOR WEST LONDON LINE, NORTH LONDON LINE, GREAT WESTERN MAIN LINE, CROSSRAIL and HS2 plus WEST COAST MAIN LINE and MIDLAND MAIN LINE

There is a lack of detailed and complete information in Network Rail's London & South East Route Utilisation Strategy regarding demand on the West London Line and others in the area that would arise from an interchange with the Great Western Main Line (GWML), Crossrail and High Speed 2 (HS2) at Old Oak Common that would also be integrated with the regeneration proposals in this area. We have therefore set out below our suggested revisions to TfL's proposed Option A for the layout for the Old Oak Common interchange.

- Our suggestions should accommodate anticipated increased rail demand arising generally, from growth on the WLL, WLL and other lines' connections with Crossrail/GWML and HS2, and the need to produce a Premier Interchange that is 'future-proofed' until at least 2080.
- We believe these suggestions are workable and provide better rail capacities and integration, with lesser impacts on residents and ecological areas, and with no sprawl.
- Our suggested curves are no tighter than elsewhere in Option A; ideally these should be eased to minimise 'wheel squeal' and running times.
- The feasibility of our suggestions in terms of gradients and height clearances for running tracks would need to be checked.

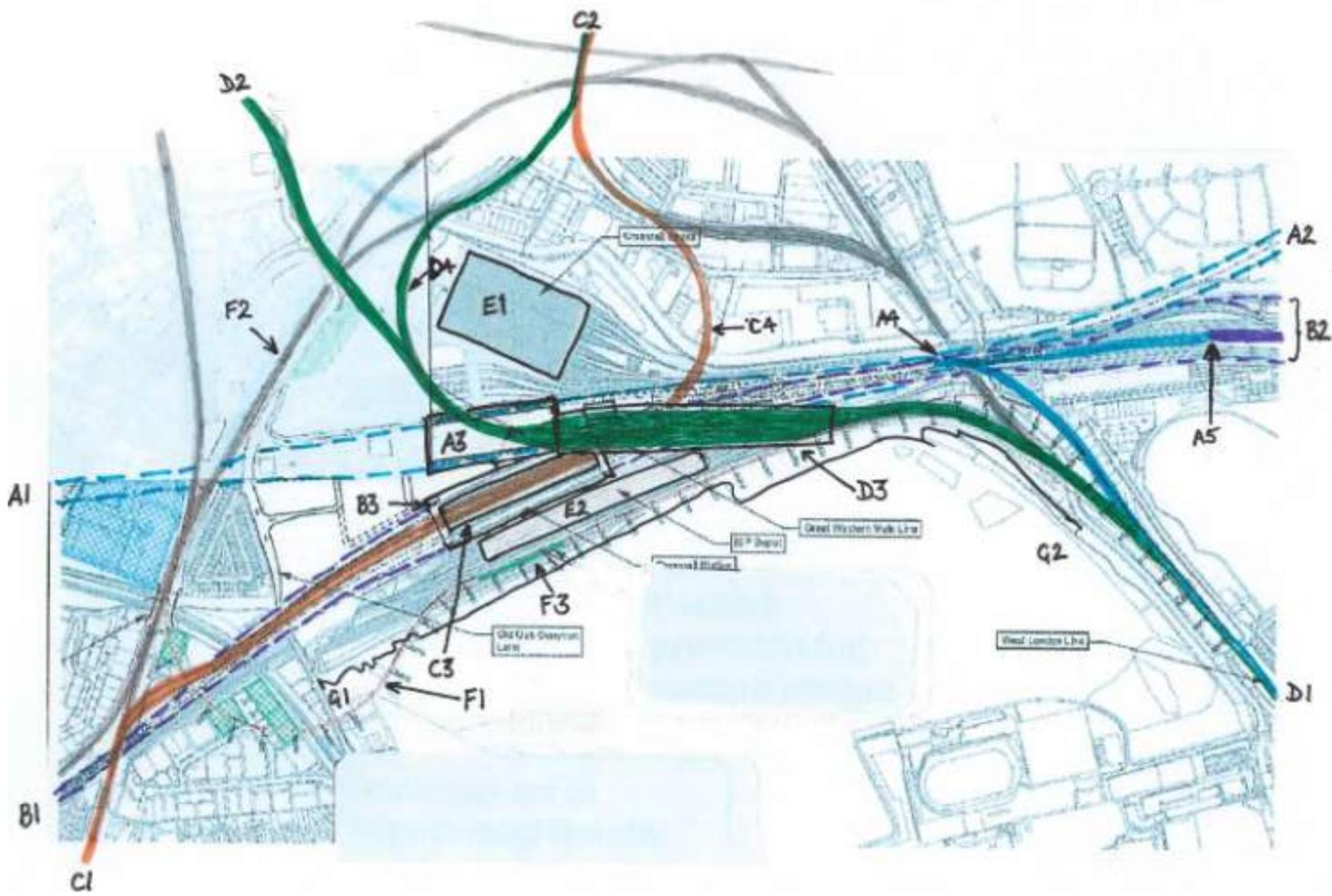
The HS2 and Crossrail/GWML platforms are unchanged, although we would urge that the HS2 platforms are elevated to reduce vertical interchange distances between them and other lines (and presumably the spoil to be removed for the HS2 station box): this elevation should also ease our suggested connections between the HS2 tracks and those of Crossrail and the WLL.

We have suggested inverting the southern approaches to Willesden Junction of the WLL and North London Line (NLL) by putting them on different axes across the site, with each running roughly east-west above the platforms for HS2 (lowest level 1) and Crossrail/GWML (level 2) to Mitre Bridge and Acton Wells Junction respectively.

We have shown the NLL platforms (level 3) directly above the Crossrail/GWML platforms and crossed by the WLL at level 4, so that the WLL would be able to clear the bulk of the Crossrail depot with minimal or no impact on the latter, although there may be other reasons why the NLL should cross above the WLL. In either case, we suggest that, to minimise adverse environmental impacts, the complex is 'tree screened' along the north of Wormwood Scrubs.

We have suggested – as an absolute minimum – a four 12-car platform layout for the WLL station box (level 4) and a twin/island 10- or 12-car layout for the NLL platforms (level 3) directly above the station box for the GWML/Crossrail. Immediately west of the WLL station box, there would be a new viaduct to allow Southern and South West Trains to reach, via the Dudding Hill Line, the West Coast Main Line and the Midland Main Line respectively.

Suggested layout for Old Oak Common



Key

A	HS2 (Light Blue)	E	Depots
A1 –	Alignment of HS2	E1	Crossrail Depot
A2	HS2 station box (Level 1)	E2	IEP Depot
A3	HS2-WLL Junction (Old Oak Common – Westway Circus)		
A4	HS2-Crossrail Junction (Denham Parkway – Paddington)		
A5			
B	Crossrail (Purple)	F	Facilities no longer needed
B1 –	Alignment of Crossrail	F1	Proposed WLL diversion no longer needed
B2	Crossrail station box (Level 2) surmounted by NLL platforms (Level 3)	F2 and F3	Proposed stations no longer needed
B3			
C	NLL (Orange)	G	Mitigation Measures
C1 –	Diverted route of NLL (Level 3) between Acton Wells and Willesden Junction	G1 – G2	Proposed line of tree-screening
C2	NLL twin/island 10-car platforms (Level 3) over Crossrail station box (Level 2)		
C3	Curve to be eased if possible		
C4			
D	WLL (Green)		
D1 –	Diverted route of WLL (Level 4) between Westway Circus and Willesden Junction		Other items
D2	Diverted route of WLL (Level 4) between Westway Circus and WCML and MML via Dudding Hill Line		All other existing railway lines to remain in situ.
D3	WLL four 12-car platforms (Level 4) over HS2 (Level 1), Crossrail (Level 2) and NLL (Level 3)		
D4	Curve to be eased if possible		

Suggested train service patterns at Old Oak Common

We envisage an intensive set of services to and from the WLL and NLL platforms in order that:

- i. the new regeneration and opportunity areas in the immediate environs are properly served with minimal impact on the existing environment arising from the station;
- ii. the WLL and NLL can fully serve Old Oak Common to meet the interchange demands here of GWML, Crossrail and HS2;
- iii. pressure is lessened on the five tube lines at Euston/Euston Square and on other parts of the Underground;
- iv. by changing at Old Oak Common an almost infinite number of routes can be taken between many different parts of this country, facilitating a modal shift from roads such as the M25; and
- v. there would be improved access for Londoners and others to UK airports, with direct trains to Heathrow, Gatwick, Luton, Stansted and Birmingham.

Suggested train services at Old Oak Common

<p>WLL</p> <ul style="list-style-type: none"> • Clapham Junction – OOC – Stratford (10-car 4tph) [London Overground] • Brighton/Gatwick – OOC – Milton Keynes (12-car 2tph) [Southern] • High Wycombe – OOC – Clapham High St – SE London/Ebbsfleet (10-car 2tph) [Chiltern/London Overground] • Bedford/Luton – OOC – Clapham Junction – Guildford (10/12-car 2tph) [FCC/SWT] • Northampton – OOC – Clapham Junction (12-car 1tph) [London Midland] • Oxford – OOC – Clapham Junction (12-car 1tph) [FGW] 	<p>NLL</p> <ul style="list-style-type: none"> • Richmond – OOC – Stratford (10-car 4tph) [London Overground] • Basingstoke/Heathrow – Hounslow – OOC – Stansted (10/12-car 2tph) [Greater Anglia/SWT]
<p>HS2</p> <ul style="list-style-type: none"> • Glasgow/Manchester – OOC – Euston • Birmingham – South Midlands – OOC – Euston • Edinburgh/Leeds/Sheffield – Toton (Nott'm/Derby) – OOC – Euston 	<p>Crossrail</p> <ul style="list-style-type: none"> • Reading/M Maidenhead/Heathrow/Denham – OOC – Central London – Shenfield/Abbey Wood/Ebbsfleet



June 2014

SUGGESTED LAYOUT and SERVICES FOR OLD OAK COMMON FOR WEST LONDON LINE, NORTH LONDON LINE, GREAT WESTERN MAIN LINE, CROSSRAIL and HS2 plus WEST COAST MAIN LINE and MIDLAND MAIN LINE

OLD OAK COMMON STATION CHECKLIST

The West London Line Group has prepared the following two-page checklist as an aide-memoire for those concerned with the production of rail interchange facilities at Old Oak Common. This should be read in conjunction with the Group's proposals for Old Oak Common, which are attached to the list.

Our proposals will achieve:-	Our proposals will avoid:-
Swiftest possible transit for passengers and trains on all lines (HS2, Crossrail, GWML, NLL, WLL) with, as far as possible, all tracks following straight lines and WLL Overground trains only stopping once and not twice and/or reversing on site	Major disturbance to:- <ul style="list-style-type: none"> existing/planned track formations DfT/HS2 'hard lines' around proposed stations, Crossrail and IEP depots
Ability for all lines to be used by double-deck passenger and Continental freight trains, fed by overhead line equipment	
A single, logically-designed, attractive and efficient station block accommodating all lines	Demolition of residential buildings
Minimum passenger interchange distances and times between all lines and the local neighbourhood	
Ideally greater economies of scale and value for money (VFM), in terms of cost of design and construction by having one all-inclusive station, and not three separate ones	Adverse impact on ecological sites, especially Wormwood Scrubs
Collaboration in imaginative and interlocking designs for the station, platform complex and rail crossing nexus	
Through the retention of existing alignments, greater rail capacity for holding and diversionary purposes, particularly to aid freight movements through the area	Conflict with long-established statutes, especially those relating to Wormwood Scrubs
Through the installation of minimal but key additional trackwork, the possibilities of new rail services, e.g., <ul style="list-style-type: none"> HS2 – HS1 via WLL, Clapham Junction (for Southern England from Devon to Sussex), East Croydon, Merstham (for M23/M25 and Gatwick), Tonbridge to Ashford (for HS1) 	

<ul style="list-style-type: none"> • HS2 (Denham) – Crossrail (West End, City and Docklands); • WLL (south) and NLL (south) to Midland Main Line (north); and • OOC to all London Airports, plus Birmingham Airport 	
<p>Addition (with no greater time penalties than those inherent in present TfL Options) of WLL station at Westway Circus to:-</p> <ul style="list-style-type: none"> • serve the White City OA and the new Imperial College complex; and • achieve other key local non-rail transport objectives 	Millions of wasted person-hours by WLL and NLL users that will be 'built in' under all other proposed configurations
A physical and practical focus for regeneration in inner west London, and especially in four Opportunity Areas (Park Royal, Old Oak, White City and Earl's Court OAs)	
The proper servicing of HS2 and Crossrail through full integration with local rail services	Excessive disturbance for local residents from 'wheel-squeal' from NLL and WLL trains
Improved access to/from Heathrow and other London and Birmingham airports for Londoners and others	
<p>Greater awareness and acceptance of, and support for, High Speed Rail by more people, including:-</p> <ul style="list-style-type: none"> • Local residents; • Londoners; • Others across the UK; and • Visitors from abroad 	<p>Unnecessary pressure at Euston on:-</p> <ul style="list-style-type: none"> • Existing rail station • Five Underground Lines • Bus and taxi services • Local area and services
Greater use of Domestic High Speed Rail, especially if HS2 includes three intermediate stations (Chiltern Ridge, Claydon (for East West Rail) and North Warwickshire)	
Greater use of International High Speed Rail, through HS2-HS1 link via Clapham Jn, Merstham and Ashford	Excessive disruption in about 2040 when attempts will need to be made to improve four stations – all in a very constrained location beneath a new 'mini-city' and all less than 10-15 years old – that will no longer be "fit for purpose".
Improved prospects of meeting demand up to 2080	
Improved footfall/financial returns for on-station businesses	

MLB
1 June 2014



June 2014

SUGGESTED LAYOUT and SERVICES FOR OLD OAK COMMON FOR WEST LONDON LINE, NORTH LONDON LINE, GREAT WESTERN MAIN LINE, CROSSRAIL and HS2 plus WEST COAST MAIN LINE and MIDLAND MAIN LINE

OLD OAK COMMON STATION CHECKLIST

The West London Line Group has prepared the following two-page checklist as an aide-memoire for those concerned with the production of rail interchange facilities at Old Oak Common. This should be read in conjunction with the Group's proposals for Old Oak Common, which are attached to the list.

Our proposals will achieve:-	Our proposals will avoid:-
Swiftest possible transit for passengers and trains on all lines (HS2, Crossrail, GWML, NLL, WLL) with, as far as possible, all tracks following straight lines and WLL Overground trains only stopping once and not twice and/or reversing on site	Major disturbance to:- <ul style="list-style-type: none"> existing/planned track formations DfT/HS2 'hard lines' around proposed stations, Crossrail and IEP depots
Ability for all lines to be used by double-deck passenger and Continental freight trains, fed by overhead line equipment	
A single, logically-designed, attractive and efficient station block accommodating all lines	Demolition of residential buildings
Minimum passenger interchange distances and times between all lines and the local neighbourhood	
Ideally greater economies of scale and value for money (VFM), in terms of cost of design and construction by having one all-inclusive station, and not three separate ones	Adverse impact on ecological sites, especially Wormwood Scrubs
Collaboration in imaginative and interlocking designs for the station, platform complex and rail crossing nexus	
Through the retention of existing alignments, greater rail capacity for holding and diversionary purposes, particularly to aid freight movements through the area	Conflict with long-established statutes, especially those relating to Wormwood Scrubs
Through the installation of minimal but key additional trackwork, the possibilities of new rail services, e.g., <ul style="list-style-type: none"> HS2 – HS1 via WLL, Clapham Junction (for Southern England from Devon to Sussex), East Croydon, Merstham (for M23/M25 and Gatwick), Tonbridge to Ashford (for HS1) 	

<ul style="list-style-type: none"> • HS2 (Denham) – Crossrail (West End, City and Docklands); • WLL (south) and NLL (south) to Midland Main Line (north); and • OOC to all London Airports, plus Birmingham Airport 	
<p>Addition (with no greater time penalties than those inherent in present TfL Options) of WLL station at Westway Circus to:-</p> <ul style="list-style-type: none"> • serve the White City OA and the new Imperial College complex; and • achieve other key local non-rail transport objectives 	Millions of wasted person-hours by WLL and NLL users that will be 'built in' under all other proposed configurations
A physical and practical focus for regeneration in inner west London, and especially in four Opportunity Areas (Park Royal, Old Oak, White City and Earl's Court OAs)	
The proper servicing of HS2 and Crossrail through full integration with local rail services	Excessive disturbance for local residents from 'wheel-squeal' from NLL and WLL trains
Improved access to/from Heathrow and other London and Birmingham airports for Londoners and others	
<p>Greater awareness and acceptance of, and support for, High Speed Rail by more people, including:-</p> <ul style="list-style-type: none"> • Local residents; • Londoners; • Others across the UK; and • Visitors from abroad 	<p>Unnecessary pressure at Euston on:-</p> <ul style="list-style-type: none"> • Existing rail station • Five Underground Lines • Bus and taxi services • Local area and services
Greater use of Domestic High Speed Rail, especially if HS2 includes three intermediate stations (Chiltern Ridge, Claydon (for East West Rail) and North Warwickshire)	
Greater use of International High Speed Rail, through HS2-HS1 link via Clapham Jn, Merstham and Ashford	Excessive disruption in about 2040 when attempts will need to be made to improve four stations – all in a very constrained location beneath a new 'mini-city' and all less than 10-15 years old – that will no longer be "fit for purpose".
Improved prospects of meeting demand up to 2080	
Improved footfall/financial returns for on-station businesses	

MLB

1 June 2014



17 June 2014

WEST LONDON LINE GROUP CALLS FOR IMPROVED CRITERIA SELECTION AT OLD OAK COMMON

This Group believes that the regeneration needed in key areas of the country and especially for inner and outer London is best delivered through the fullest possible integration between nation's rail networks.

Proper access to and interchange between these networks will also allow more of those who will pay for and/or suffer the impacts of these projects – along with their friends, associates and families – to make full use of these national facilities. Thus comprehensive and effective interchange with existing rail routes should be a principal aim of all headline transport projects such as Crossrail and HS2.

This is why opportunities of hubs such as Old Oak Common for regeneration and transport must be fully seized.

We believe that there should be easy, adequate and seamless interchange between all lines that converge on Old Oak Common, including (and this may not be an exhaustive list) GWML, NLL, WLL, Crossrail1 and HS2.

In assessing each option for this interchange, more emphasis should be put on elements such as the quality of the passenger experience, the minimum total journey minutes, the number of route options available, the opportunities to introduce other services, plus the ability of the infrastructure under assessment to expand to meet growth rates up to three times those usually forecast for at least 25 years.

'Passenger experience' should cover such areas as the above; plus:-

- i. the speeds with which users can move between different lines and/or the outside of the station;
- ii. the convenience to passengers and potential rail users of having multiple connections at a single site;
- iii. the number of person-years lost by using a particular, possibly tortuous, rail routing;
- iv. the opportunity for more robust and comprehensive passenger information arrangements (especially when services are disrupted); and
- v. a more inclusive, positive attitude of staff, operators and other relevant organizations towards all users of the interchange.

Other areas that should also be given sufficient weight are:-

- i. the economies of scale and overall BCR and VFM arising from each option;
- ii. the adverse impacts on local residents, e.g., from construction, train and station noise and light spill, and costs and satisfactoriness of mitigation;

- iii. the adverse impacts on the local ecology and costs and satisfactoriness of mitigation;
- iv. legal and other difficulties in securing new wayleaves (e.g., Wormwood Scrubs)

We also believe that in determining best options for route alignments, track connections and platform positioning, the principal *'raison d'etre'* of the rail network should be paramount, i.e., that it is where it is primarily to serve passengers and freight, on *trajets* that are as straight and swift as possible.

Thus, while we fully appreciate that depots, sidings and rail engineering works are important adjuncts to the main networks, we would contend that these should be secondary to ensuring the greatest efficiency in passenger and freight flows by rail.

We therefore sincerely hope that those in charge of such developments will exercise the necessary flexibility to meet the over-arching goals of efficient rail movement – optimum passenger and freight handler satisfaction and a healthier economy and happier public thereby. In the case of Old Oak Common, we would hope that this could extend, if necessary, to agreed encroachment or re-drawing of site boundaries, and even to re-designing or re-building structures which may impact on desired rail alignments.

MLB
17 June 2014



Department
for Transport

Mark Balaam
Chairman
West London Line Group

Via email

Sean Delaney
HS2 Sponsor Team
Department for Transport
Great Minster House
33 Horseferry Road
London
SW1P 4DR
Tel: 020 7944 3530

Email –
sean.delaney@dft.gsi.gov.uk

Web Site: www.gov.uk/dft

Our Ref: TO 107762
Your Ref:

DATE 4 July 2014

Dear Mr Balaam

Further to our recent meeting and my subsequent email update, please be advised that HS2 Ltd have now requested its design consultants to initiate a process to determine the scope, cost and programme to undertake some high-level spatial assessment work to establish the feasibility of incorporating any new Overground designs for OOC.

HS2 Ltd are currently awaiting a response from their design consultants. I have instructed HS2 Ltd to contact you when they have an update on this matter.

Yours sincerely,

Sean Delaney

Richard Morris – HS2 Ltd

Two views from the West London Line showing its immediate proximity to the area which will be occupied by the new station at Old Oak Common for HS2, Crossrail and the Great Western Main Line station, but with no interchange or connection with the WLL yet planned.

